



Alabama Port Authority



Project Name Choctaw Point Container Terminal – Phase V Dock Extension

Location Mobile, Alabama

Project # 11411

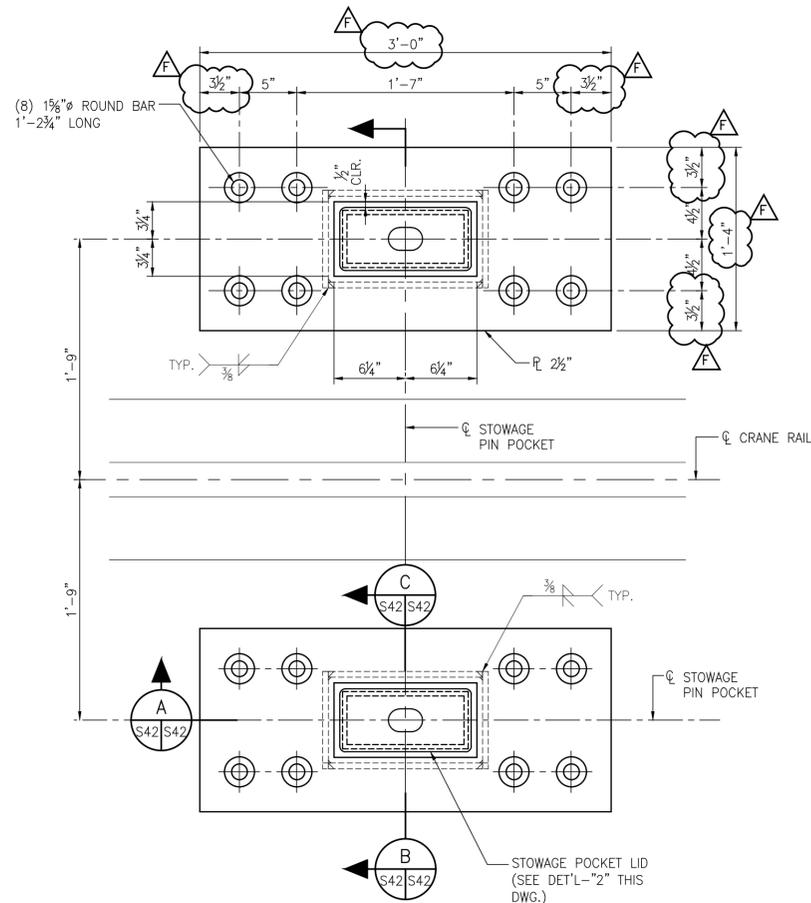
Task # 6

March 11, 2026

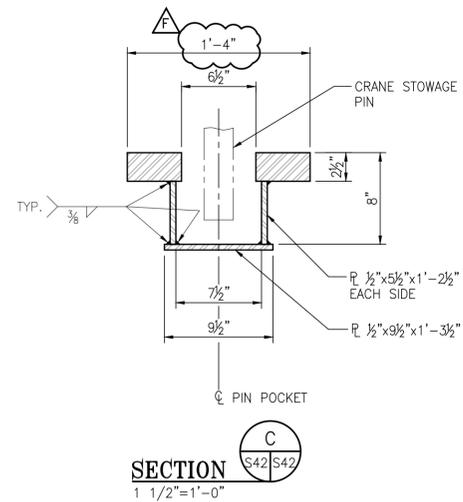
Addendum 2
1 | Page

ADDENDUM NO. 2

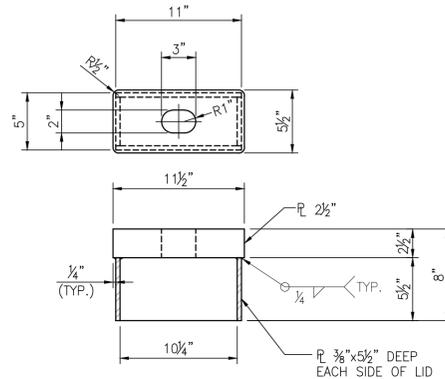
Item	Description
Effective March 9, 2026, the Port’s legal name has been changed to Alabama Port Authority. The name change bill, Act 2026-217, was signed by the Governor on March 9, 2026, officially establishing the name change. Moving forward all official documents will use the name Alabama Port Authority, or reference “the Alabama Port Authority, formerly known as the Alabama State Port Authority.”	
DIVISION I – BID DOCUMENTS	
No additions	
DIVISION II – CONTRACT DOCUMENTS	
No additions	
DIVISION III – SPECIAL PROVISIONS	
No additions	
DIVISION IV – GENERAL PROVISIONS	
No additions	
DIVISION V – CONSTRUCTION SPECIFICATIONS	
No additions	
NOTES	
1	Drawing Nos. 4617 S42, 4617 D6, 4617 D7 (attached)
2	Contractor Questions



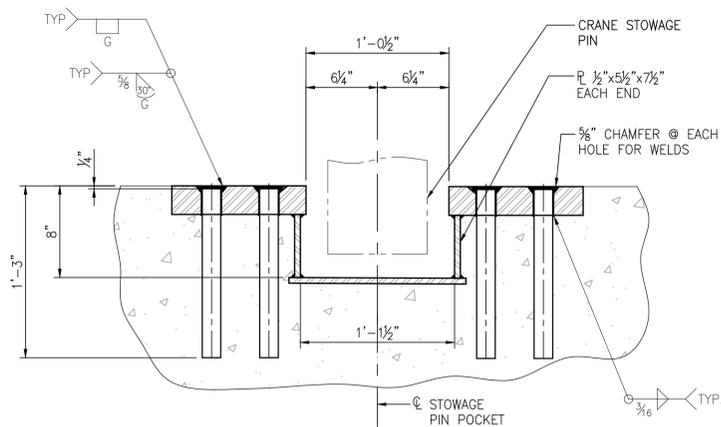
DETAIL 1
S35, S40, S41, S41A, S42
CRANE STOWAGE PIN POCKETS
1 1/2"=1'-0" (2 REQ'D. PER STOWAGE LOCATION)



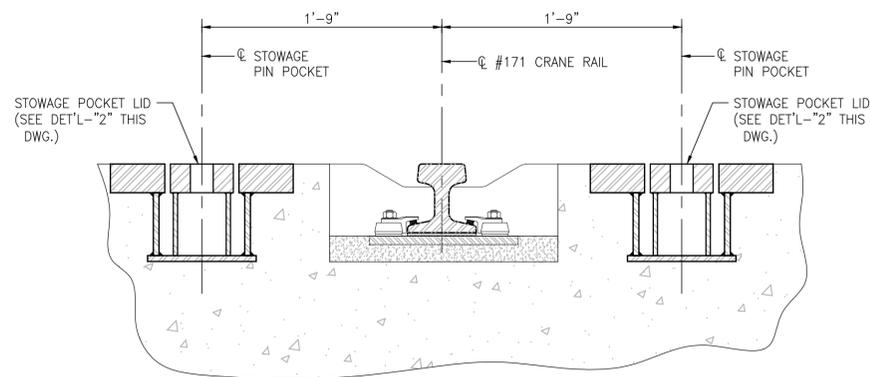
SECTION C
S42, S42
1 1/2"=1'-0"



DETAIL 2
S42, S42
CRANE STOWAGE PIN POCKET LID
1 1/2"=1'-0" (2 REQ'D. PER STOWAGE LOCATION)



SECTION A
S42, S42
1 1/2"=1'-0"



SECTION B
S42, S42
1 1/2"=1'-0"

NOTES

- SEE DRAWING G2 FOR PROJECT GENERAL NOTES, SURVEY NOTES & DRAWING INDEX.
- SEE DRAWING S1 FOR STRUCTURAL GENERAL NOTES.
- ALL MATERIAL UNLESS NOTED OTHERWISE, SHALL BE A572 GRADE 50, HOT-DIP GALVANIZE AFTER FABRICATION.

F	ADDENDUM 2 - CHANGED PL 2 1/2" LENGTH & WIDTH DIMENSIONS	RST	MED	RSG	03-05-26
E	ISSUED FOR BID	RST	MED	RSG	01-14-26
D	100% DESIGN REVIEW	RST	MED	RSG	09-09-25
C	95% DESIGN REVIEW	RST	MED	RSG	08-08-25
B	60% DESIGN REVIEW	RST	MED	RSG	06-10-25
A	30% DESIGN REVIEW	ECW	JEJ	RSG	04-11-25
NO.	REVISION	DRAWN	CK'D	APPD	DATE

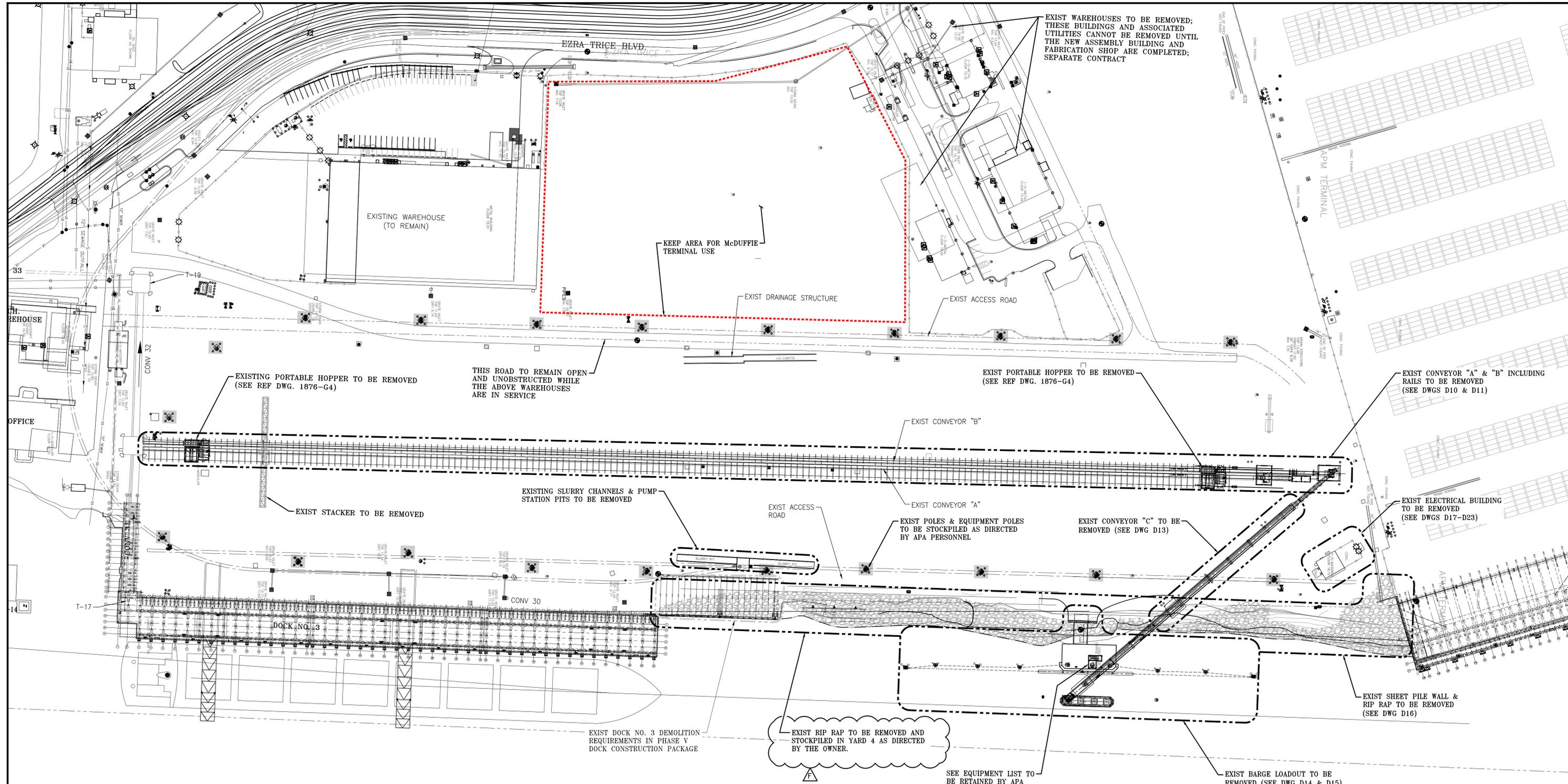


CLIENT DWG NO:
**CHOCTAW POINT CONTAINER TERMINAL
PHASE V DOCK EXTENSION
W.S. & L.S. STOWAGE PIN POCKET DETAILS**

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FOR: **ALABAMA PORT AUTHORITY**

DRAWN: RST	CK'D: JEJ	APPD: RSG	DATE: 08-21-24	SCALE: AS NOTED	PROJECT NO: 4617	DWG NO: S42	REV: F
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- YARD 4 ADDITIONAL EQUIPMENT LIST TO BE REMOVED BY THE CONTRACTOR AND RETAINED BY APA (SUBJECT TO CHANGE):**
- (2) WINCH ASSEMBLIES ON EXISTING BARGE LOADER (WINCH, MOTOR, GEARBOX, & BRAKE).
 - (4) CABLE REEL ASSEMBLIES FROM EXISTING EQUIPMENT: 2 RAIL HOPPERS, BI-WING, & BARGE LOADER.
 - (2) MCC FROM RAIL HOPPERS: 1 ON EACH HOPPER.
 - (2) ROOFTOP AIR CONDITIONING UNITS FROM ELECTRICAL BUILDING.
 - (2) MOTORS & GEAR BOXES FROM EXISTING CONVEYOR "C".
 - TOWER STRUCTURE WITH STAIRS AT BARGE LOADER.

MOBILE RIVER
 EXISTING SITE PLAN
 1"=80'-0"

NOTES
1. SEE DRAWING G2 FOR DEMOLITION NOTES, DRAWING INDEX, AND REFERENCE DRAWING INDEX.
2. TYPICAL DEMOLITION SECTIONS AND DETAILS AND ATTACHED REFERENCE DRAWINGS ARE FOR INFORMATION ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING ACTUAL EXISTING SITE CONDITIONS PRIOR TO SUBMITTING THEIR BID.
3. ALL EXISTING MATERIAL REMOVED DURING DEMOLITION, INCLUDING BUT NOT LIMITED TO STRUCTURAL STEEL, RAILS, CONCRETE, PILES, MOORING SYSTEMS & FENDERS, ETC., SHALL BE DISPOSED OF OFF SITE UNLESS NOTED OTHERWISE.

NO.	REVISION	DATE	APPD	CK'D	DRAWN	DATE
F	ADDENDUM 2 REVISED NOTE	03-11-26	RST	JEU	RSG	03-11-26
E	ISSUED FOR BID	01-14-26	JLH	JEU	RSG	01-14-26
D	100% DESIGN REVIEW	09-09-25	ECW	JEU	JLH	09-09-25
C	95% DESIGN REVIEW	08-08-25	JLH	JEU	RSG	08-08-25
B	60% DESIGN REVIEW	06-10-25	ECW	JEU	RSG	06-10-25
A	30% DESIGN REVIEW	04-11-25	ECW	JEU	RSG	04-11-25

thompson
ENGINEERING

APTIM
PORT SERVICES, LLC

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ALABAMA
PORT AUTHORITY
PORT OF MOBILE

CLIENT DWG NO:

CHOCTAW POINT CONTAINER TERMINAL
 PHASE V DOCK EXTENSION - DEMOLITION PACKAGE
 EXISTING SITE PLAN

FOR: ALABAMA PORT AUTHORITY

PROJECT NO: 4617 DWG NO: D6 REV: F



EXIST WAREHOUSES TO BE REMOVED; THESE BUILDINGS AND ASSOCIATED UTILITIES CANNOT BE REMOVED UNTIL THE NEW ASSEMBLY BUILDING AND FABRICATION SHOP ARE COMPLETED; SEPARATE CONTRACT

EXISTING WAREHOUSE (TO REMAIN)

KEEP AREA FOR McDUFFIE TERMINAL USE

EXIST DRAINAGE STRUCTURE

EXIST ACCESS ROAD

EXISTING PORTABLE HOPPER TO BE REMOVED (SEE REF DWG. 1876-G4)

THIS ROAD TO REMAIN OPEN AND UNOBSTRUCTED WHILE THE ABOVE WAREHOUSES ARE IN SERVICE

EXIST PORTABLE HOPPER TO BE REMOVED (SEE REF DWG. 1876-G4)

EXIST CONVEYOR "A" & "B" INCLUDING RAILS TO BE REMOVED (SEE DWGS D10 & D11)

EXIST STACKER TO BE REMOVED

EXISTING SLURRY CHANNELS & PUMP STATION PITS TO BE REMOVED

EXIST CONVEYOR "B"

EXIST ACCESS ROAD

EXIST CONVEYOR "A" TO BE STOCKPILED AS DIRECTED BY APA PERSONNEL

EXIST CONVEYOR "C" TO BE REMOVED (SEE DWG D13)

EXIST ELECTRICAL BUILDING TO BE REMOVED (SEE DWGS D17-D23)

EXIST DOCK NO. 3 DEMOLITION REQUIREMENTS IN PHASE V DOCK CONSTRUCTION PACKAGE

EXIST RIP RAP TO BE REMOVED AND STOCKPILED IN YARD 4 AS DIRECTED BY THE OWNER.

SEE EQUIPMENT LIST TO BE RETAINED BY APA

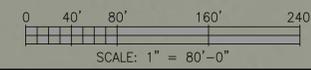
EXIST BARGE LOADOUT TO BE REMOVED (SEE DWG D14 & D15)

EXIST SHEET PILE WALL & RIP RAP TO BE REMOVED (SEE DWG D16)

- YARD 4 ADDITIONAL EQUIPMENT LIST TO BE REMOVED BY THE CONTRACTOR AND RETAINED BY APA (SUBJECT TO CHANGE):**
- (2) WINCH ASSEMBLIES ON EXISTING BARGE LOADER (WINCH, MOTOR, GEARBOX, & BRAKE).
 - (4) CABLE REEL ASSEMBLIES FROM EXISTING EQUIPMENT: 2 RAIL HOPPERS, BI-WING, & BARGE LOADER.
 - (2) MCC FROM RAIL HOPPERS: 1 ON EACH HOPPER.
 - (2) ROOFTOP AIR CONDITIONING UNITS FROM ELECTRICAL BUILDING.
 - (2) MOTORS & GEAR BOXES FROM EXISTING CONVEYOR "C".
 - TOWER STRUCTURE WITH STAIRS AT BARGE LOADER.



EXISTING SITE PLAN - AERIAL OVERLAY
1"=80'-0"



- NOTES**
- SEE DRAWING G2 FOR DEMOLITION NOTES, DRAWING INDEX, AND REFERENCE DRAWING INDEX.
 - TYPICAL DEMOLITION SECTIONS AND DETAILS AND ATTACHED REFERENCE DRAWINGS ARE FOR INFORMATION ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING ACTUAL EXISTING SITE CONDITIONS PRIOR TO SUBMITTING THEIR BID.
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REFERENCE DRAWINGS	NO.	REVISION	APPD	DATE	DRAWN	CK'D	APPD	DATE
	F	ADDENDUM 2 REVISED NOTE	RST	02-11-26	JEU	RSG		
	E	ISSUED FOR BID	JLH	01-14-26	JEU	RSG		
	D	100% DESIGN REVIEW	ECW	09-09-25	JEU	RSG		
	C	95% DESIGN REVIEW	JLH	08-08-25	JEU	RSG		
	B	60% DESIGN REVIEW	ECW	06-10-25	JEU	RSG		
	A	30% DESIGN REVIEW	ECW	04-11-25	JEU	RSG		



CLIENT DWG NO:
CHOCTAW POINT CONTAINER TERMINAL
PHASE V DOCK EXTENSION - DEMOLITION PACKAGE
EXISTING SITE PLAN - AERIAL OVERLAY

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FOR: **ALABAMA PORT AUTHORITY**

DRAWN: ECW CK'D: JEU APPD: RSG DATE: 01-03-25 SCALE: AS NOTED PROJECT NO: **4617** DWG NO: **D7** REV: **F**

**11411- Task 6 – Choctaw Point Container Terminal – Phase V Dock
Extension
Contractor Questions**

1. Bid Specification Section 412200 Crane Rails, Section 2.1 indicates that the rail is to be supplied Head Hardened per ASTM A759, S2. The only producing rail mill of this profile capable of producing true Head Hardened Rails per ASTM A759, S2 permanently closed its facility (Cleveland-Cliffs in Steelton, PA) as of June 2025. The 171 lb/yd Profile is now only produced in Europe by ArcelorMittal and they do not Head Hardened Crane Rails. Will the EOR please confirm that the rail can be supplied in the new European Grade of 110 Cr-V, which is the closest equivalent and meets the 321 min BHN requirement.
 - The 171 lb/yd crane rail with steel grade 110 Cr-V is acceptable.

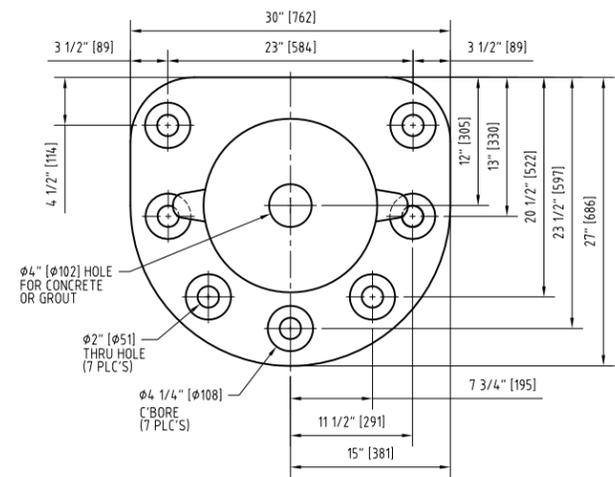
2. Bid Specification Section 412200 Crane Rails Section 2.4.2 and Bid Drawing S45, Note 5 indicates that all of the Crane Rail is to be Thermite Welded. Will the EOR approve for use Flash-Welding the rail joints, as long as the Flash-Welding Contractor has a Pre-Qualified Program in accordance with AREMA Chapter 4 and AWS D15.2? Flash-Welded Rail Joints are the highest quality joint and this is the preferred method recommended for all new installations.
 - Flash welding of the rail joints as stated would be acceptable. It is expected that the rail joints would have to be thermite welded at the rail expansion joint locations.

3. Bid Specification Section 260543.1, Section 1.2 and 2.1 indicate that the Cable Trench is to be made or supplied by a specific company. Would the EOR or Owner accept a comparable system from another supplier meeting the same performance criteria and dimensional requirements? We could provide domestic references for similar scope projects.
 - The owner and engineer may accept an alternative that is equal to or better than that specified. Any alternative cable trench must also adequately interface with the existing Panzerbelt system currently installed at the container terminal.

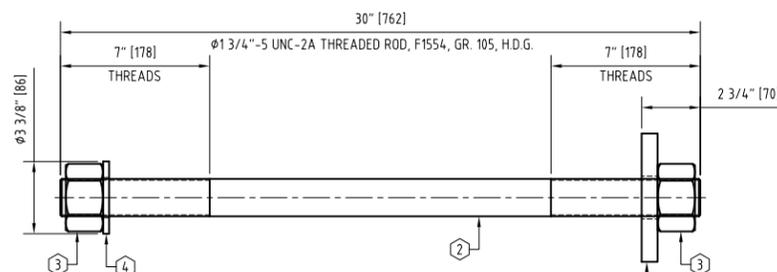
4. On drawing D16 there is a sheet pile wall that is to be removed. This wall currently appears to be completely underwater. Could you please provide a top of wall elevation and tip elevation, so that we may see if it interferes with batter pile installation?
 - As shown in the reference drawings, top of wall is at elevation (+) 2.0 feet and the tip elevation is at (-) 60 feet.

5. On drawings D6, D7, and D14 there appear to be additional sheet pile walls, one with a concrete cap, that will also need removal. Could you please provide a top of wall elevation and tip elevation, so that we may see if they interfere with batter pile installation? These walls are also shown on reference drawings NC1, NC2, and G6.
 - Walls were existing and the only specific information is the top of cap on drawing G6 indicates elevation of (+) 13.4'.
6. Could you please provide a location on a general arrangement drawing for the soil boring shown on D18? The material appears to be quite dense at this location.
 - Boring is in the location of the electrical substation building that is to be removed.
7. Will the APA provide a certificate showing that the coal is non-hazardous?
 - APA is expecting to have documentation on this by mid March.
8. The scope of work calls for the riprap currently on the slopes and out in the river to be removed and stored at the Logistics Park on Baker Street. Will the contractor be responsible for separating the driftwood and trash that is currently accumulated on the slopes from the riprap? In addition, can the driftwood be burned at the Logistics Park location?
 - The Contractor shall separate the rip rap from the trash, however, the rip rap can remain in Yard 4 and stockpiled in a location designated by the Owner. The trash is to be hauled off site and not burned.
9. Will the attached 100 and 250 ton single bitt bollards from Trelleborg be approved as bollard alternatives?
 - We take no exception to the submitted bollards (Trelleborg Drawings MSB-100 and MSB-250 Bollard Plans (pages 1 & 2) attached)
10. Mobile falls under the FO exposure class, which calls for no additional air entrainment outside of what is occurring naturally within the mix.
 - The Concrete specifications will remain as is. The cited requirements state that the exposure category shall be assigned by the design professional. In this case, air entrainment is being specified for the additional durability desired due to the pier being located in a consistently moist environment.
11. Type II cement is not available in the area, only type 1L or 1S.
 - Type 1S will be acceptable.

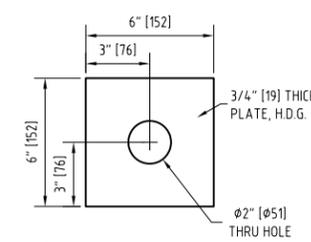
ITEM	SHEET	DESCRIPTION	QTY.	JOB QTY.
BOLLARD				
1	1	MSB-100 CAST STEEL BOLLARD	1	??
HARDWARE				
2	1	Ø1 3/4"-5 UNC-2A x 30" THREADED ANCHOR ROD, F1554, GR. 105, H.D.G.	7	??
3	1	1 3/4"-5 UNC-2B HEAVY HEX NUT, A563, H.D.G.	14	??
4	1	1 3/4" FLAT WASHER, F436, H.D.G.	7	??
STEEL FABRICATION				
5	1	3/4" THICK PLATE WASHER, A36, H.D.G.	7	??



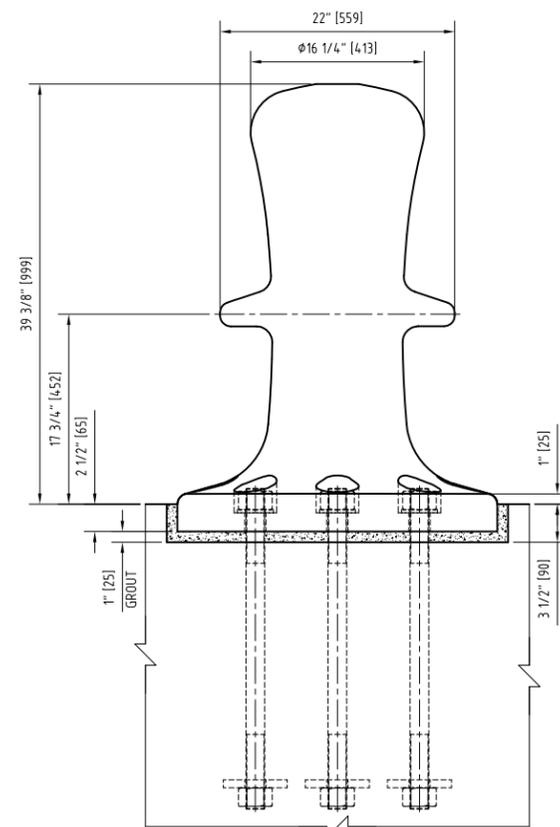
A PLAN VIEW
SCALE: 1 1/2"=1'



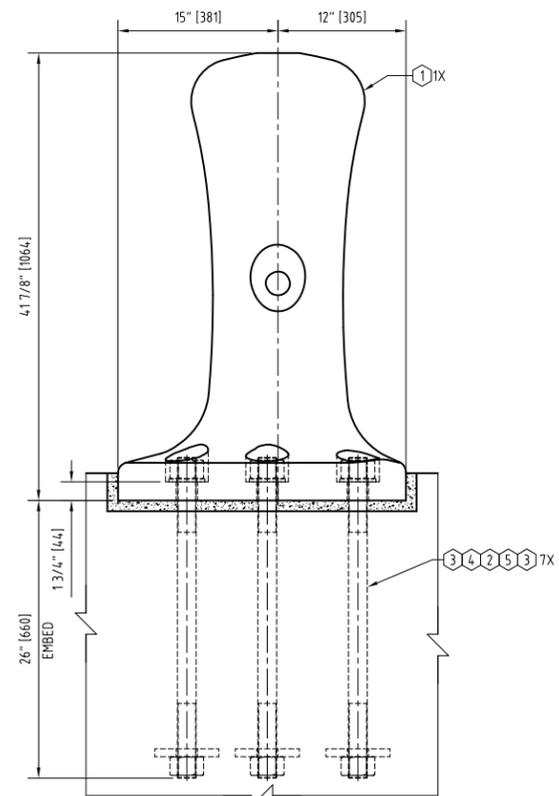
D ANCHOR ASSEMBLY
SCALE: 3"=1'
(?? REQ'D)



5 PLATE WASHER, A36
SCALE: 3"=1'
(?? REQ'D)



B FRONT ELEVATION
SCALE: 1 1/2"=1'



C SIDE ELEVATION
SCALE: 1 1/2"=1'



- GENERAL NOTES:
- BOLLARD TO BE CAST STEEL ACCORDING TO ASTM A27 GR. 65-35.
 - ALL HARDWARE TO BE HOT DIP GALVANIZED AS PER ASTM A123 OR A153 AS REQ'D.
 - BOLLARD TO HAVE ALLOWABLE LINE PULL OF 100 METRIC TONNES MAXIMUM.
 - PAINT SYSTEM FOR BOLLARD TO BE AS FOLLOWS: BLAST ALL OUTSIDE SURFACES TO SSPC 6 FINISH BEFORE APPLYING PRIMER COAT.
 - PRIMER COAT: 2.0-3.0 MILS OF CARBOZINC 11.
 - ALL DIMENSIONS ARE IN INCHES [MM].

0	XXX	INITIAL RELEASE	XX/XX/XX
REV	BY	DESCRIPTION	DATE



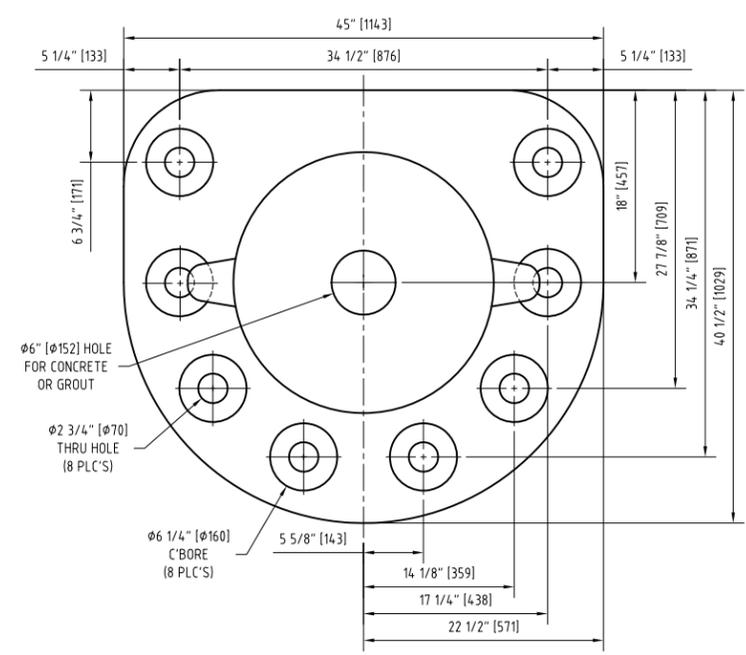
PROJECT TITLE
Trelleborg Marine Systems North America

DRAWING TITLE
MSB-100 BOLLARD PLANS

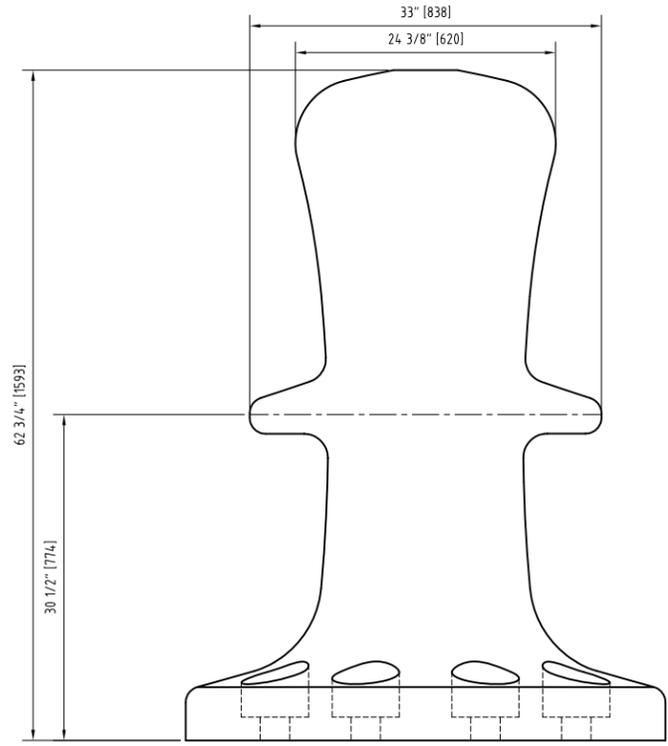
DATE	DRAWN BY	CHECKED BY	APPROVED BY
XX/XX/XX	XXX	XXXX	XXXX
SHEET	SCALE	DRAWING NO.	REV
ARCH D	AS NOTED	XXXX-XX	0

ITEM	SHEET	DESCRIPTION	QTY.	JOB QTY.
BOLLARD				
1	1	MSB-250 CAST STEEL BOLLARD	1	??

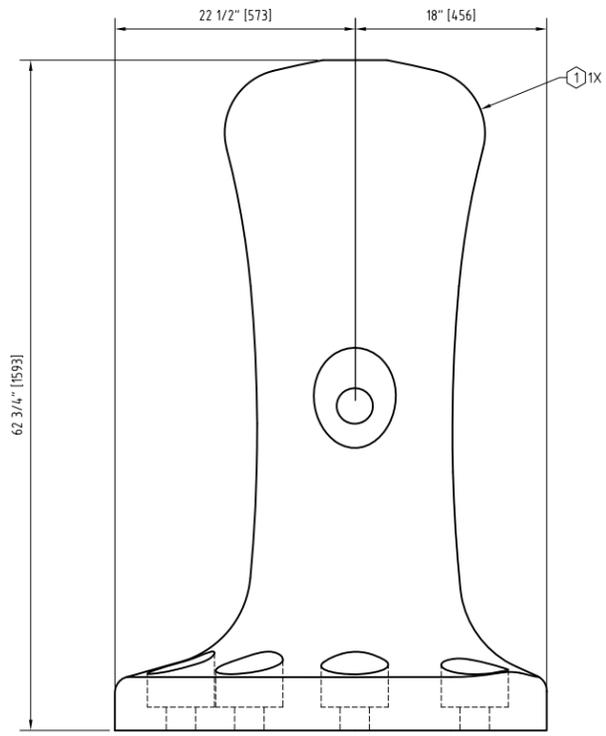
- GENERAL NOTES:
- BOLLARD TO BE CAST STEEL ACCORDING TO ASTM A27 GR. 65-35.
 - BOLLARD TO HAVE ALLOWABLE LINE PULL OF 250 METRIC TONNES MAXIMUM.
 - PAINT SYSTEM FOR BOLLARD TO BE AS FOLLOWS:
BLAST ALL OUTSIDE SURFACES TO SSPC 6 FINISH BEFORE APPLYING PRIMER COAT.
a. PRIMER COAT: 2.0-3.0 MILS OF CARBOZINC 11.
 - ALL DIMENSIONS ARE IN INCHES [MM].



A PLAN VIEW
SCALE: 1 1/2"=1'



B FRONT ELEVATION
SCALE: 1 1/2"=1'



C SIDE ELEVATION
SCALE: 1 1/2"=1'



0	XXX	INITIAL RELEASE	XX/XX/XX
REV	BY	DESCRIPTION	DATE



PROJECT TITLE
Trelleborg Marine Systems North America

DRAWING TITLE
MSB-250 BOLLARD PLANS

DATE	DRAWN BY	CHECKED BY	APPROVED BY
XX/XX/XX	XXX	XXXX	XXXX
SHEET	SCALE	DRAWING NO.	REV
ARCH D	AS NOTED	XXXX-XX	0
		PROJECT NO.	XXXX